

21 March 2024

File Ref: OIAPR-1274023063-25756

[REDACTED]  
[REDACTED]

Tēnā koe [REDACTED]

### **Request for information 2024-037**

I refer to your request for information dated 05 March 2024, which was received by Greater Wellington Regional Council (Greater Wellington) on 06 March 2024. You have requested the following:

“Under the LGOIMA I request the business case (and/or similar documents) for the upgrades to bus stops 7236 and 6236 (slightly south of the Rotherham Tce and Park Road intersections in Miramar). These upgrades were completed in late 2023.”

### **Greater Wellington’s response follows:**

The changes made to these stops were for operational accessibility improvements, so there was no business case made.

Therefore, we have refused this part of your request under section 17(e) on the basis that that the document alleged to contain the information requested does not exist or, despite reasonable efforts to locate it, cannot be found.

### **Background:**

The improvements carried out to the layout of bus stops 6236 and 7236 on Park Road were originally identified as being needed around 2019 when the new bus operator contracts took effect. The introduction of larger buses, including double deckers, highlighted the deficiencies with stop 6236 in particular. The issue with this stop is buses were unable to align properly with the kerb due to the presence of a large Pohutukawa tree that meant buses were stopping within the live traffic lane to let down passengers, which was neither safe nor accessible for customers. The issue with stop 7236 was the position of a large power pole towards the rear of the stop that became an issue with buses that have a wide tail swing. While temporary measures were introduced to avoid damage to buses as an interim measure, designs seen in **Attachment 1** were prepared, and

construction of the improvements carried out in 2023 with funds allocated through Council's 2021-2024 Long Term Plan.

If you have any concerns with the decision(s) referred to in this letter, you have the right to request an investigation and review by the Ombudsman under section 27(3) of the Local Government Official Information and Meetings Act 1987.

Please note that it is our policy to proactively release our responses to official information requests where possible. Our response to your request will be published shortly on Greater Wellington's website with your personal information removed.

Nāku iti noa, nā



**Samantha Gain**

Kaiwhakahaere Matua Waka-ā-atea | Group Manager Metlink

# GWRC - MINOR CIVIL WORKS PARK ROAD BUS STOP

## DRAWINGS INDEX

DWG No.	DRAWING TITLE
310205101-02-001-C000	COVER SHEET, INDEX AND LOCALITY PLAN
310205101-02-001-C002	SITE 1 GENERAL ARRANGEMENT BUS STOP #6236 - 89A PARK ROAD
310205101-02-001-C003	SITE 2 GENERAL ARRANGEMENT BUS STOP #7236 - 86 PARK ROAD
310205101-02-001-C010	SITE 1 TRAFFIC SERVICES AND SET OUT BUS STOP #6236 - 89A PARK ROAD
310205101-02-001-C011	SITE 2 TRAFFIC SERVICES AND SET OUT BUS STOP #7236 - 86 PARK ROAD
310205101-02-001-C020	SITE 1 TYPICAL SECTIONS BUS STOP #6236 - 89A PARK ROAD
310205101-02-001-C021	SITE 2 TYPICAL SECTIONS BUS STOP #7236 - 86 PARK ROAD
310205101-02-001-C023	KERB AND PAVEMENT DETAIL
310205101-02-001-C030	STANDARD DETAILS SHEET 1 OF 2
310205101-02-001-C031	STANDARD DETAILS SHEET 2 OF 2
310205101-02-001-C040	VEHICLE TRACKING

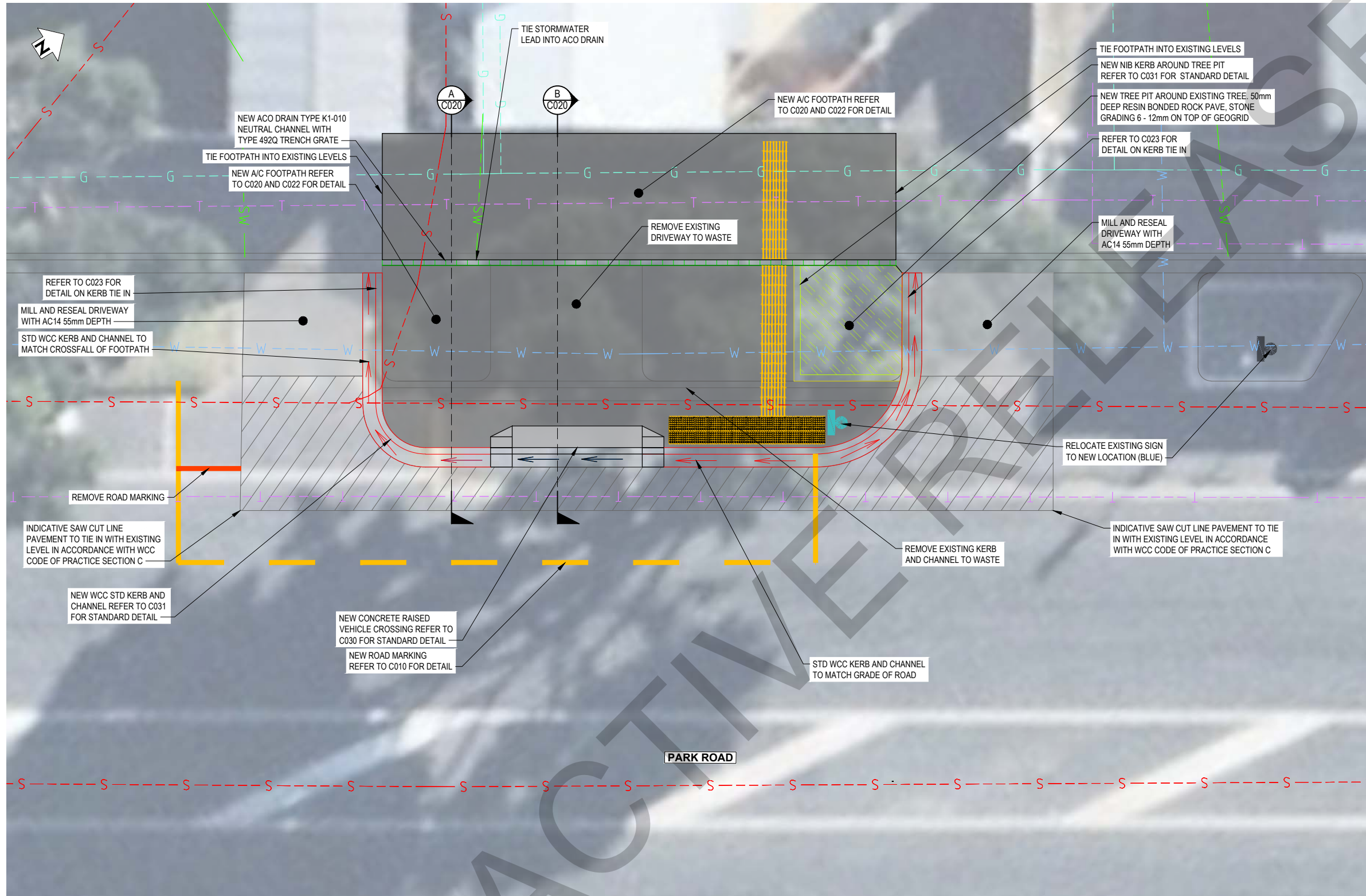
LOCALITY PLAN - NOT TO SCALE



**FOR CONSTRUCTION**  
 13.04.2023

DO NOT SCALE - IF IN DOUBT, ASK

ORIGINAL SIZE A1



**LEGEND**

- NEW YELLOW ROAD MARKING
- ROAD MARKING TO BE REMOVED
- NEW WCC STANDARD KERB AND CHANNEL
- NEW KASSEL KERB
- VEHICLE CROSSING
- NEW NIB KERB
- NEW ACO DRAIN
- EXISTING SIGN LOCATION
- PROPOSED SIGN LOCATION
- EXISTING KERB AND CHANNEL
- NEW ASPHALT
- PAVEMENT REINSTATEMENT
- TACTILE AND DIRECTIONAL PAVERS

**EXISTING SERVICES LEGEND**

- GAS
- POWER
- TELECOMMS
- WATER
- SEWER
- STORM WATER

- NOTES**
- ALL SIGNS AND ROAD MARKINGS TO COMPLY WITH THE WAKA KOTAHĪ MANUAL OF TRAFFIC SIGNS AND MARKINGS (MOTSAM) OR TRAFFIC CONTROL DEVICES MANUAL (TCDM) IF SUPERCEDED
  - CONFLICTING SIGNS TO BE RECOVERED OR REMOVED
  - CONFLICTING ROAD MARKINGS TO BE REMOVED BY WATER BLASTING OR OTHER METHOD APPROVED BY ENGINEER
  - ROAD MARKINGS TO BE REFLECTORISED LONG-LIFE SPECIFICATION
  - SIGN POSITIONS TO BE CONFIRMED WITH ENGINEER ON SITE
  - ALL WORKS SHALL BE IN ACCORDANCE WITH THE WELLINGTON CITY COUNCIL CODE OF PRACTICE
  - THE LOCATION OF ANY UNDERGROUND SERVICES SHOWN ARE INDICATIVE ONLY AND ALL SERVICES SHOULD BE SET OUT & LOCATED BY THE APPROPRIATE SERVICE PROVIDERS PRIOR TO ANY PHYSICAL WORKS
  - ALL DIMENSIONS IN METRES UNLESS OTHERWISE STATED

- TACTILE PAVER INSTALLATION**
- TACTILE SURFACES WILL NORMALLY BE 60mm THICK YELLOW CONCRETE TILES LAID ON A 40mm THICK 2:1 SAND/CEMENT BED.
  - TACTILE SURFACE TO COMPLY WITH LTSa RTS-14.

**PLAN**  
SCALE 1 : 50 (A1), 1 : 100 (A3)

**WARNING**  
BEWARE OF UNDERGROUND SERVICES  
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**ORIGINAL DRAWING**  
IN COLOUR

REV	FOR CONSTRUCTION	CD	BH	CDK	04 23
DRN	CHK	APP	DATE		
1	FOR CONSTRUCTION				04 23

SURVEYED	DESIGNED	DRAWN	CAD REVIEW	DESIGN CHECK	DESIGN REVIEW	APPROVED
-	Cobus de Kock	Shay Graham	Brody Halliday	Matt Hopkins	Andrew McLeod	Cobus de Kock



GWRC - MINOR CIVIL WORKS  
PARK ROAD BUS STOP

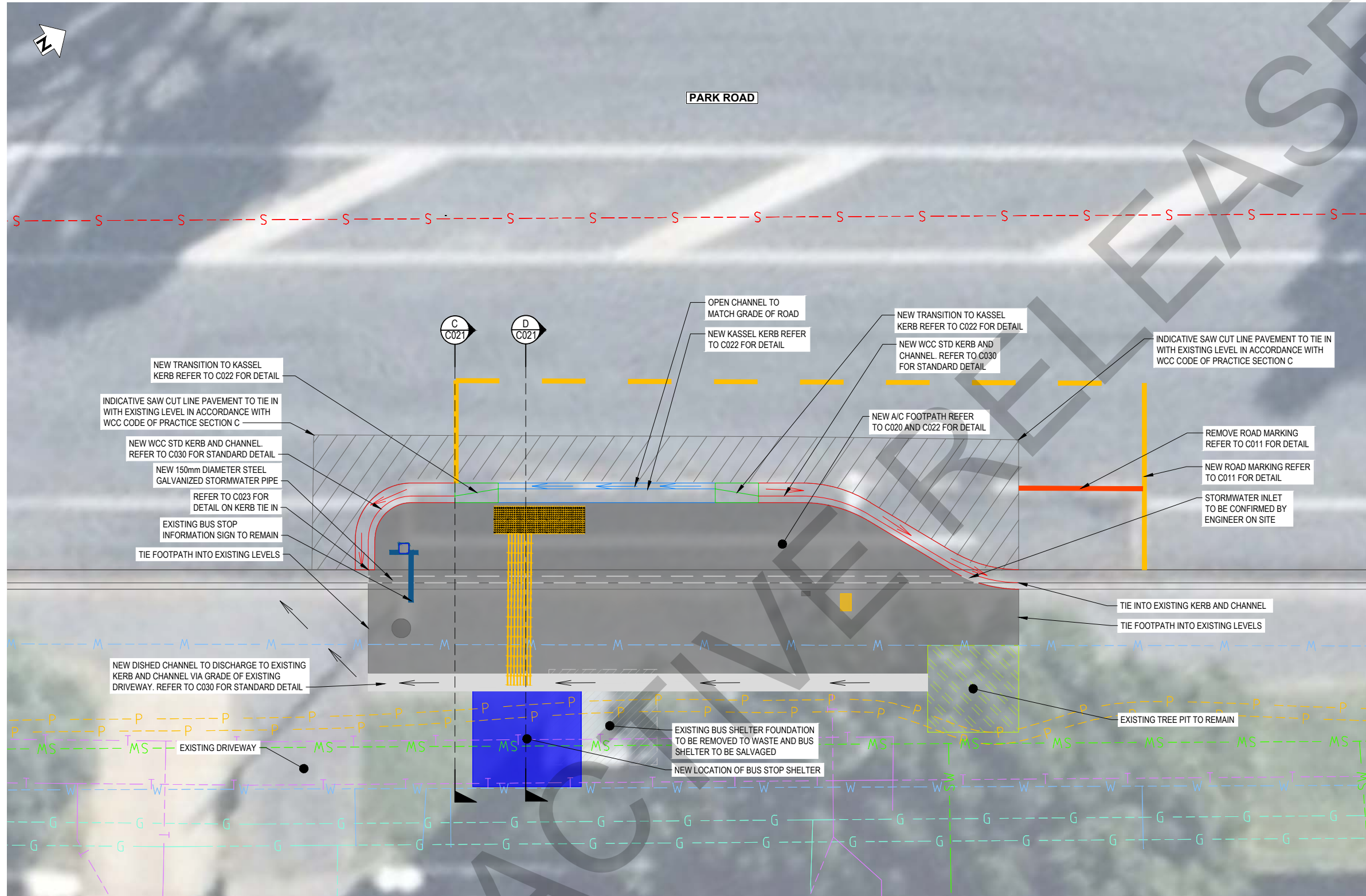
SITE 1 GENERAL ARRANGEMENT  
BUS STOP #6236 - 89A PARK ROAD

Status Stamp	<b>FOR CONSTRUCTION</b>
Date Stamp	13.04.2023
Scales	AS SHOWN
Drawing No.	310205101-02-001-C002
Rev.	1



DO NOT SCALE - IF IN DOUBT, ASK

ORIGINAL SIZE A1



**LEGEND**

- NEW YELLO ROAD MARKING
- ROAD MARKING TO BE REMOVED
- NEW WCC STANDARD KERB AND CHANNEL
- NEW KASSEL KERB
- KERB TRANSITION
- VEHICLE CROSSING
- NEW NIB KERB
- NEW ACO DRAIN
- ↑ EXISTING SIGN LOCATION
- EXISTING KERB AND CHANNEL
- NEW ASPHALT
- PAVEMENT REINSTATEMENT
- TACTILE AND DIRECTIONAL PAVERS

**EXISTING SERVICES LEGEND**

- G GAS
- P POWER
- T TELECOMMS
- W WATER
- S SEWER
- SW STORM WATER

- NOTES**
- ALL SIGNS AND ROAD MARKINGS TO COMPLY WITH THE WAKA KOTAHĪ MANUAL OF TRAFFIC SIGNS AND MARKINGS (MOTSAM) OR TRAFFIC CONTROL DEVICES MANUAL (TCDM) IF SUPERCEDED
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**IN COLOUR**

REV	DESCRIPTION	CD	BH	CDK	DATE
DRN	CHK	APP			
1	FOR CONSTRUCTION	CD	BH	CDK	04.23

STATUS	BY	DATE
SURVEYED	-	-
DESIGNED	Cobus de Kock	01/22
DRAWN	Shay Graham	01/22
CAD REVIEW	Brody Halliday	13.04.23
DESIGN CHECK	Matt Hopkins	13.04.23
DESIGN REVIEW	Andrew McLeod	13.04.23
APPROVED	Cobus de Kock	13.04.23

Client: **greater WELLINGTON REGIONAL COUNCIL**  
Te Pane Matua Taiaro

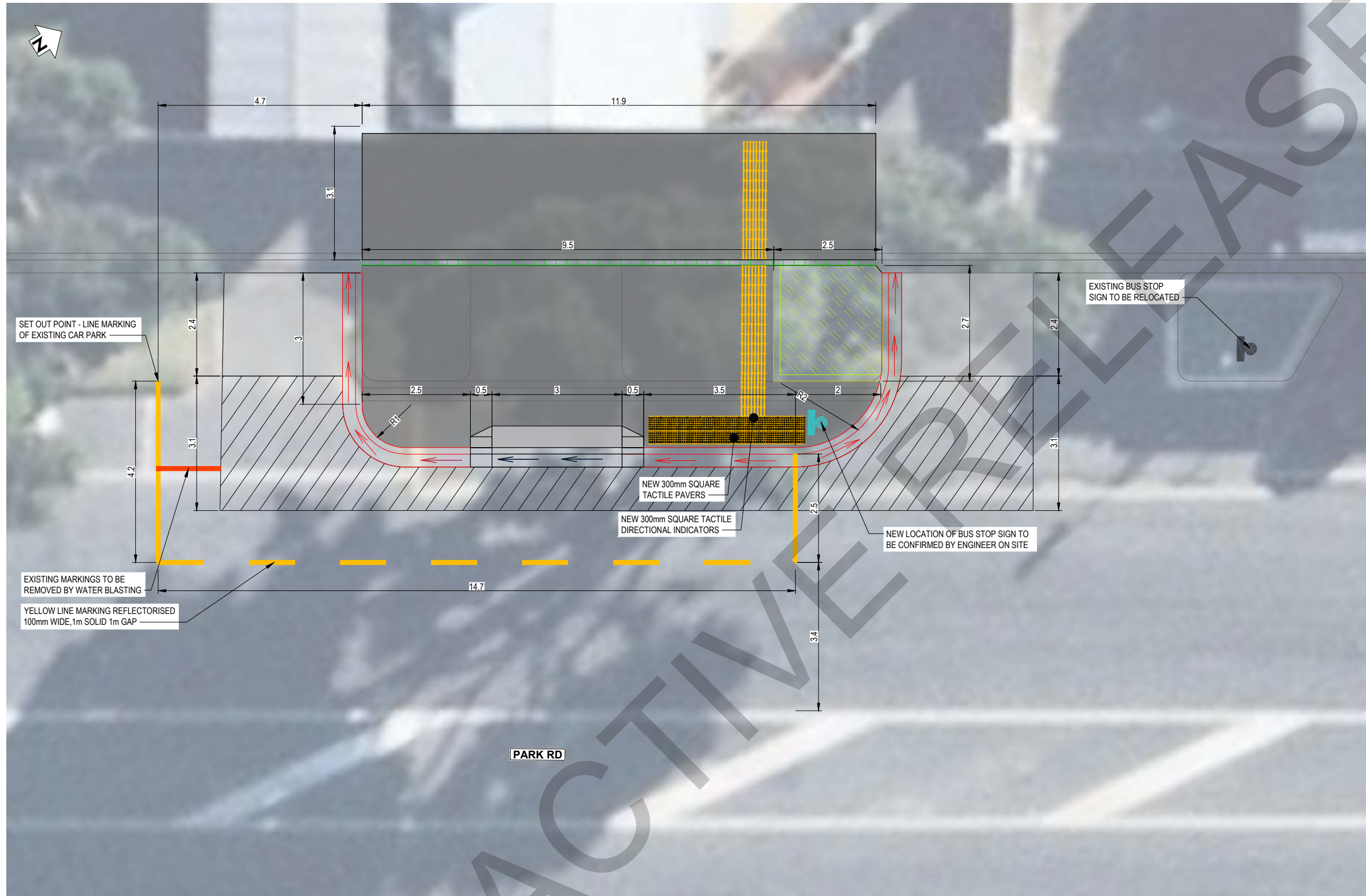
**GWRC - MINOR CIVIL WORKS**  
**PARK ROAD BUS STOP**

**SITE 2 GENERAL ARRANGEMENT**  
**BUS STOP #7236 - 86 PARK ROAD**

Status Stamp	<b>FOR CONSTRUCTION</b>
Date Stamp	<b>13.04.2023</b>
Scale	AS SHOWN
Drawing No.	310205101-02-001-C003
Rev.	1

ORIGINAL SIZE A1

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**LEGEND**

- NEW YELLOW ROAD MARKING
- ROAD MARKING TO BE REMOVED
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APPROVED	Cobus de Kock	13.04.23



GWRC - MINOR CIVIL WORKS  
PARK ROAD BUS STOP

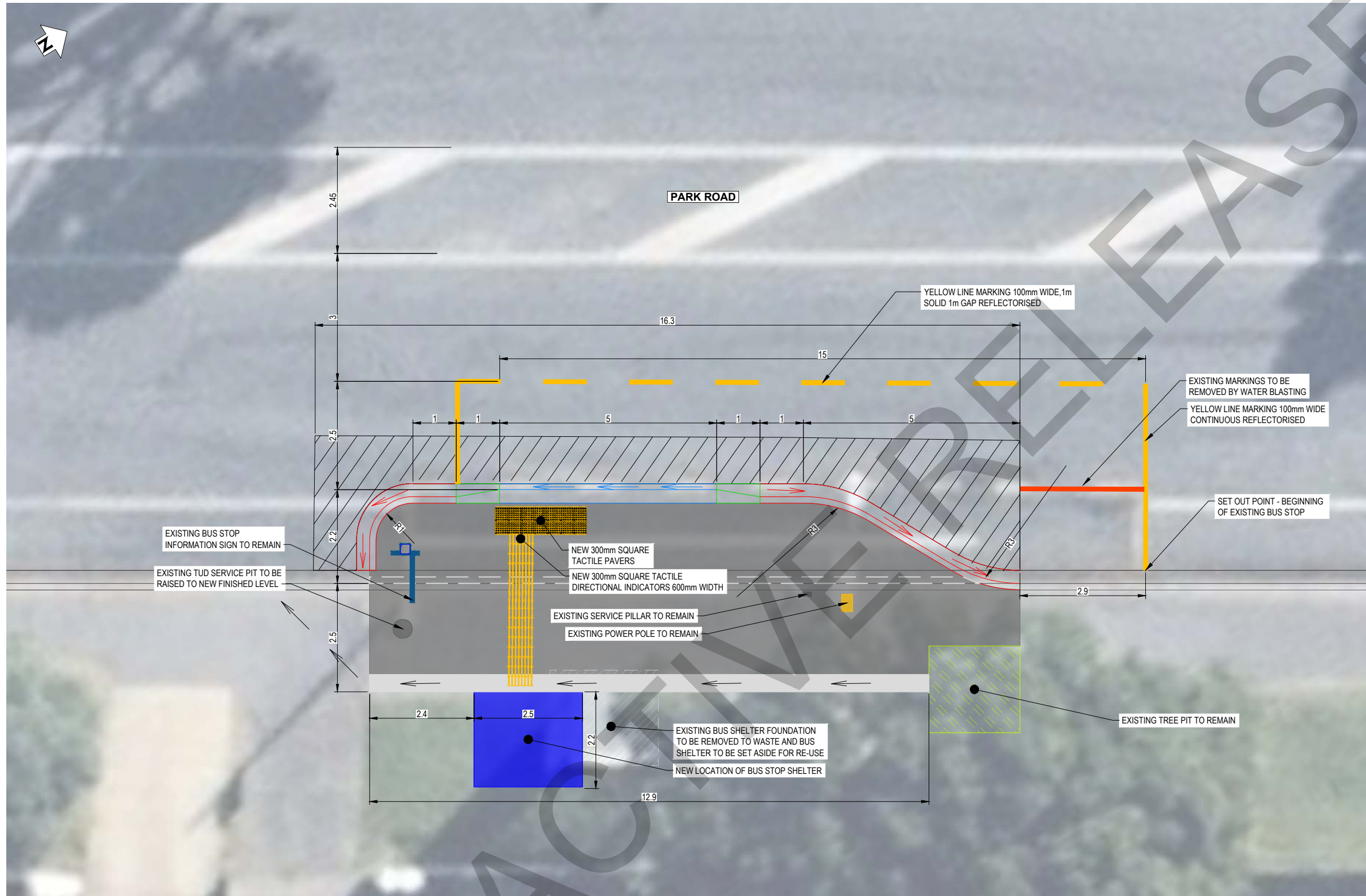
SITE 1 TRAFFIC SERVICES AND SETOUT  
BUS STOP #6236 - 89A PARK ROAD

<b>FOR CONSTRUCTION</b>	
Date Stamp	13.04.2023
Scales	AS SHOWN
Drawing No.	310205101-02-001-C010
Rev.	1



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ORIGINAL SIZE A1



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Client: **greater WELLINGTON REGIONAL COUNCIL**  
Te Pane Matusu Talao

**Stantec**

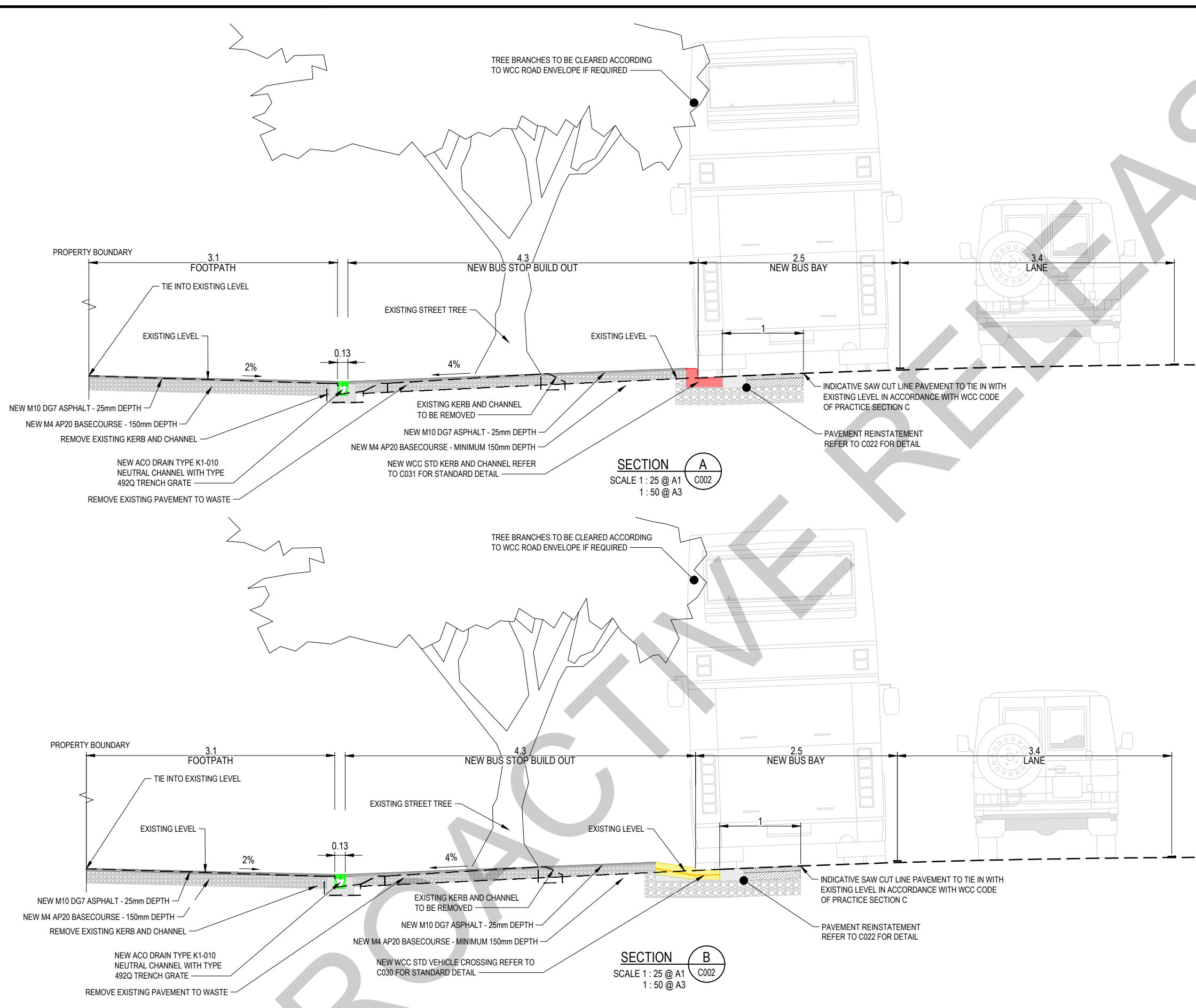
GWRC - MINOR CIVIL WORKS  
PARK ROAD BUS STOP

SITE 2 TRAFFIC SERVICES AND SETOUT  
BUS STOP #7236 - 86 PARK ROAD

Status Stamp	<b>FOR CONSTRUCTION</b>
Date Stamp	13.04.2023
Scales	AS SHOWN
Drawing No.	310205101-02-001-C011
Rev.	1

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SURVEYED		-
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DESIGN REVIEW	Andrew McLeod	13.04.23
APPROVED	Cobus de Kock	13.04.23
PROF REGISTRATION:		

Client



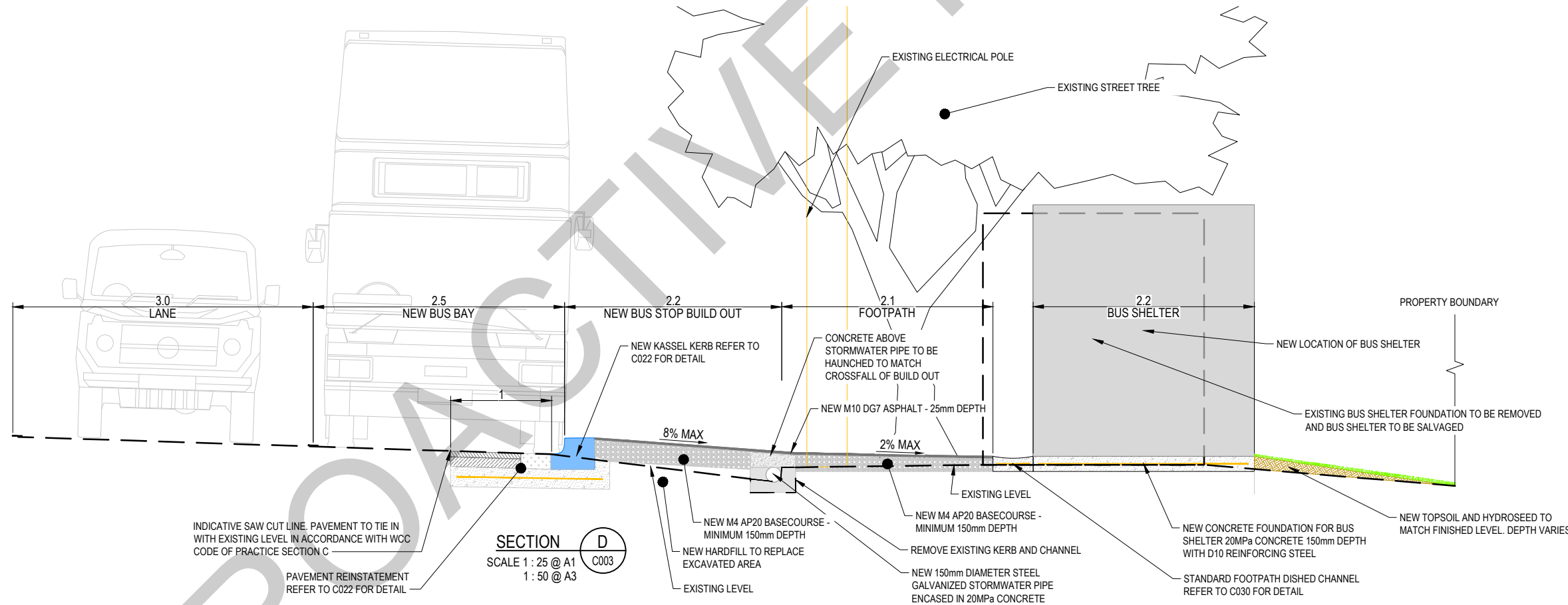
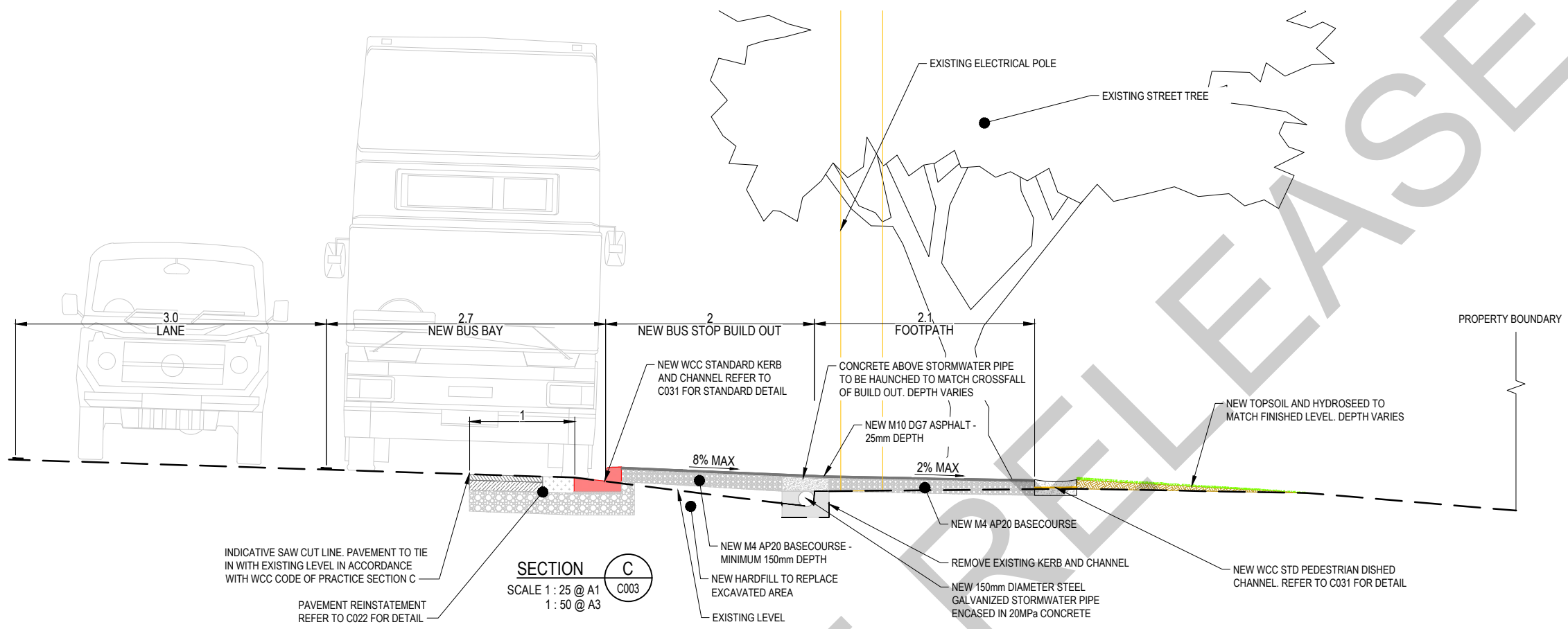

GWRC - MINOR CIVIL WORKS  
PARK ROAD BUS STOP

SITE 1 TYPICAL SECTIONS  
BUS STOP #6236 - 89A PARK ROAD

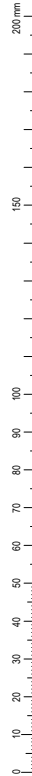
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Date Stamp	<b>13.04.2023</b>
Scales	AS SHOWN
Drawing No.	310205101-02-001-C020
Rev.	1



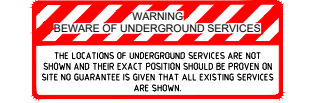
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DO NOT SCALE - IF IN DOUBT, ASK



ORIGINAL SIZE A1



**ORIGINAL DRAWING  
IN COLOUR**

REV	FOR CONSTRUCTION	CD	BH	CDK	04 23	DATE	PROF REGISTRATION:
REV	REVISIONS	DRN	CHK	APP	DATE		
1	FOR CONSTRUCTION				04 23		

SURVEYED	-	
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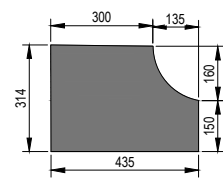
Client: **GWRC - MINOR CIVIL WORKS  
PARK ROAD BUS STOP**

**SITE 2 TYPICAL SECTIONS  
BUS STOP #7236 - 86 PARK ROAD**

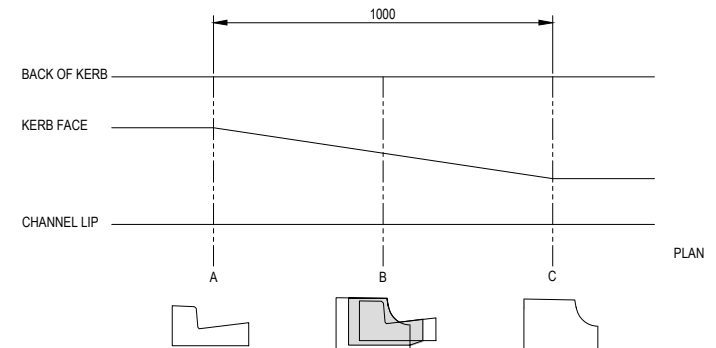
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Date Stamp	<b>13.04.2023</b>
Scales	AS SHOWN
Drawing No.	<b>310205101-02-001-C021</b>
Rev.	<b>1</b>

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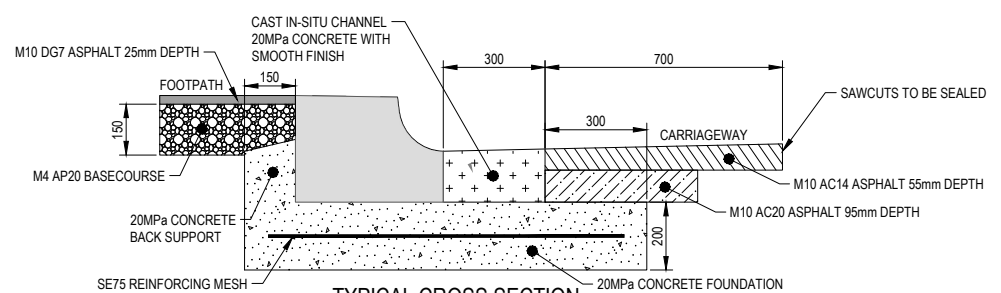
ORIGINAL SIZE A1



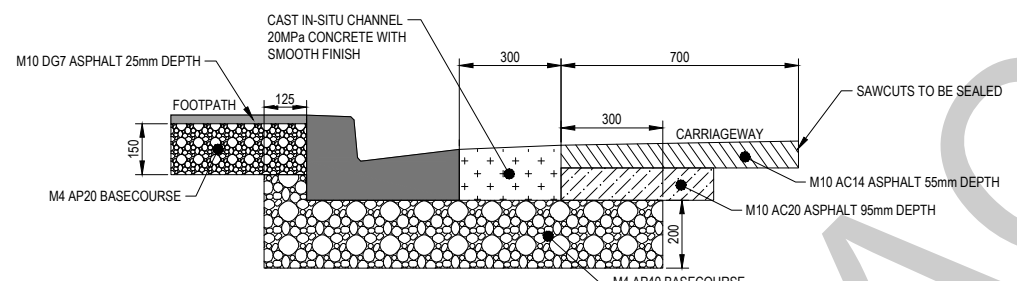
**KASSEL KERB**  
NOT TO SCALE



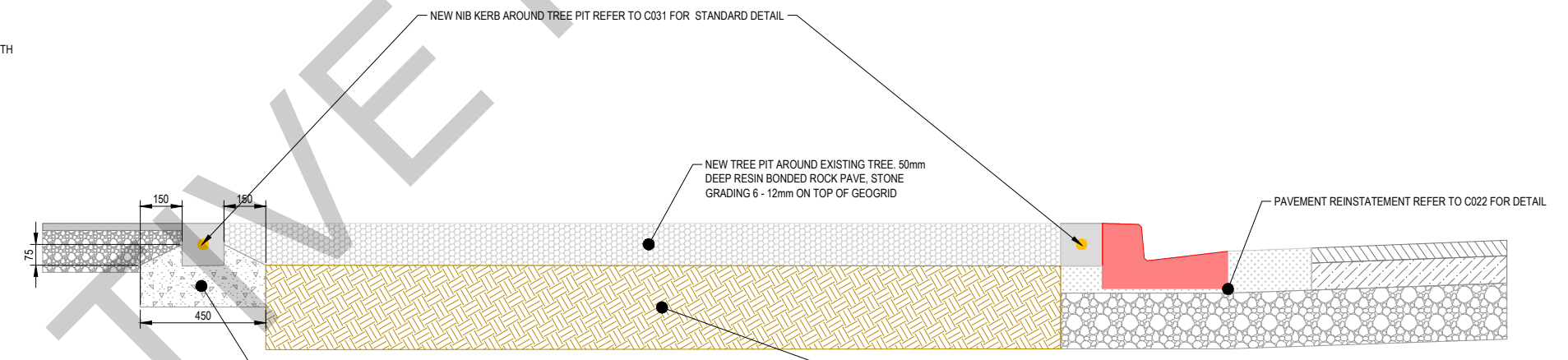
**TRANSITION BETWEEN KERB & CHANNEL AND KASSEL KERB**  
NOT TO SCALE



**TYPICAL CROSS SECTION KASSEL KERB**  
NOT TO SCALE



**TYPICAL CROSS SECTION STD KERB AND CHANNEL**  
NOT TO SCALE



**SITE ONE BUS STOP #6236 TREE PIT DETAIL**  
NOT TO SCALE

**PAVEMENT SPECIFIC REQUIREMENTS**

THE PAVEMENT MATERIALS AND CONSTRUCTION ARE TO BE CONSTRUCTED WITH THE RELEVANT WELLINGTON CITY COUNCIL SPECIFICATIONS AND NZTA SPECIFICATIONS.

THE FOLLOWING SPECIFICATIONS SHALL APPLY:

1. THE AC14 AND AC20 ASPHALT SHOULD BE IN ACCORDANCE WITH NZTA M10:2020 (HEAVY CATEGORY) WHILE THE DG7 SHOULD BE DESIGNED FOR A MEDIUM CATEGORY.
2. ASPHALT MIX DESIGN SHALL BE SUPPLIED TO STANTEC FOR CHECKING AND VERIFICATION OF THE DESIGN.
3. BINDER SHOULD BE IN ACCORDANCE WITH NZTA M1A:2019, P58-V FOR AC14 AND AC20, WHILE THE DG7 SHOULD MEET PG58-S.

**KASSEL KERB CONSTRUCTION METHODOLOGY**

THE KASSEL KERB WILL BE INSTALLED AS PER THE MANUFACTURER'S INSTALLATION INSTRUCTIONS.

THE CONSTRUCTION METHODOLOGY FOR THE PAVEMENT UNDER THE KASSEL KERB INVOLVES:

1. EXCAVATE DOWN TO A DEPTH OF 350mm BELOW THE FINAL KERB LIP LEVEL.
2. CAST A 200mm CONCRETE FOUNDATION USING 20MPa STRENGTH CONCRETE AND ADD SE75 REINFORCING MESH
3. CAST THE 20MPa CONCRETE BACK SUPPORT FOR THE KERB.
4. LAY THE KERB ON A LEVEL FOOTING, SHIMMED TO THE CORRECT HEIGHT, AS PER THE KASSEL INSTALLATION MANUAL.
5. UNDERTAKE A MEMBRANE SEAL UNDERNEATH THE AC20 AND ADD TACK COAT TO THE SURFACE AND SIDES OF THE CONCRETE BEFORE PLACING THE AC20.
6. CONSTRUCT THE 700mm WIDE AC20 UP TO 55mm BELOW THE FINAL SURFACE AND ADD A TACK COAT OVER THE AC20.
7. CONSTRUCT THE ASPHALT SURFACING AS PER THE PAVEMENT CONSTRUCTION METHODOLOGY.

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Client: **GWRC - MINOR CIVIL WORKS**  
**PARK ROAD BUS STOP**

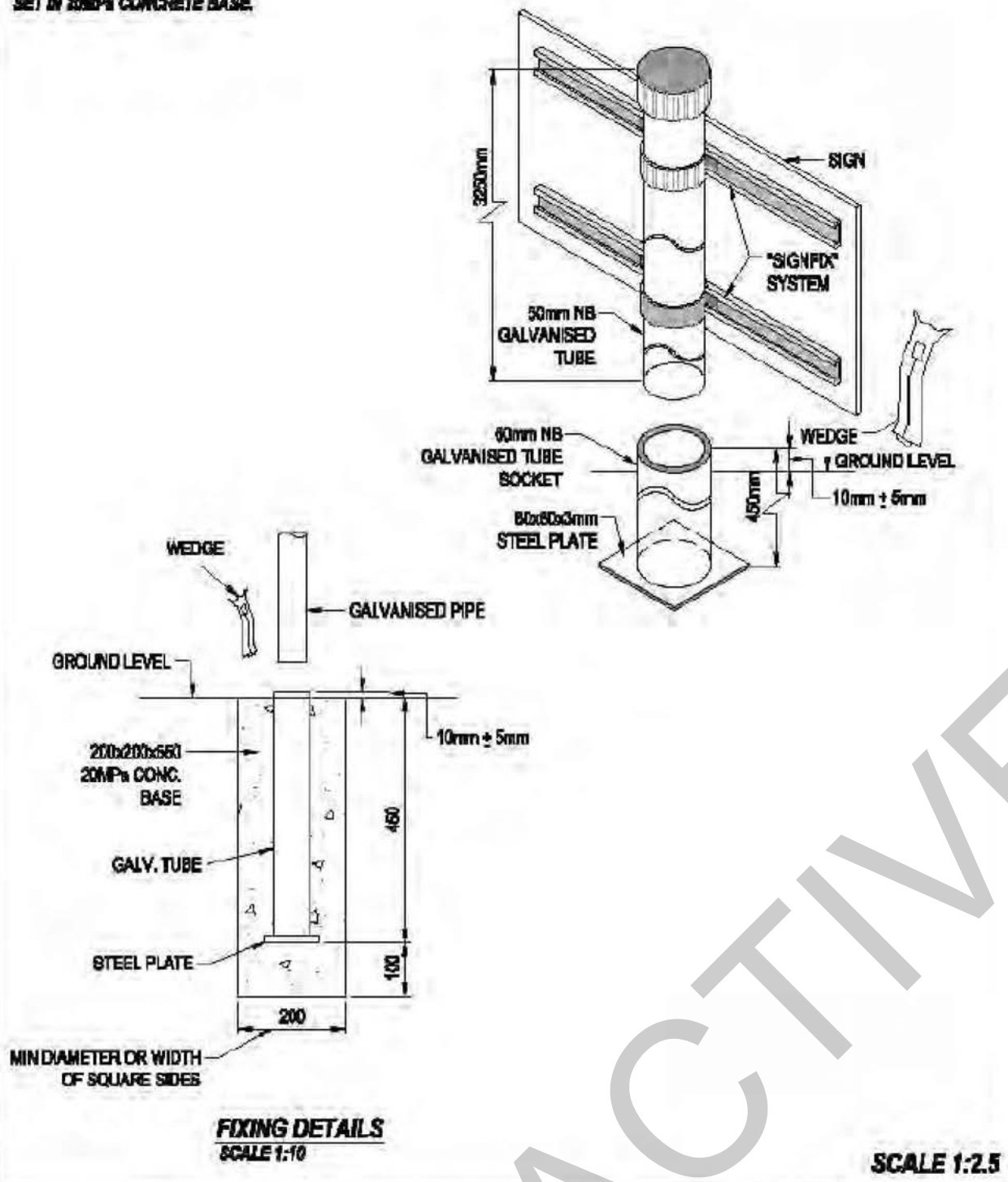
**KERB AND PAVEMENT DETAIL**

Status Stamp	<b>FOR CONSTRUCTION</b>
Date Stamp	<b>13.04.2023</b>
Scales	NOT TO SCALE
Drawing No.	<b>310205101-02-001-C022</b>
Rev.	<b>1</b>

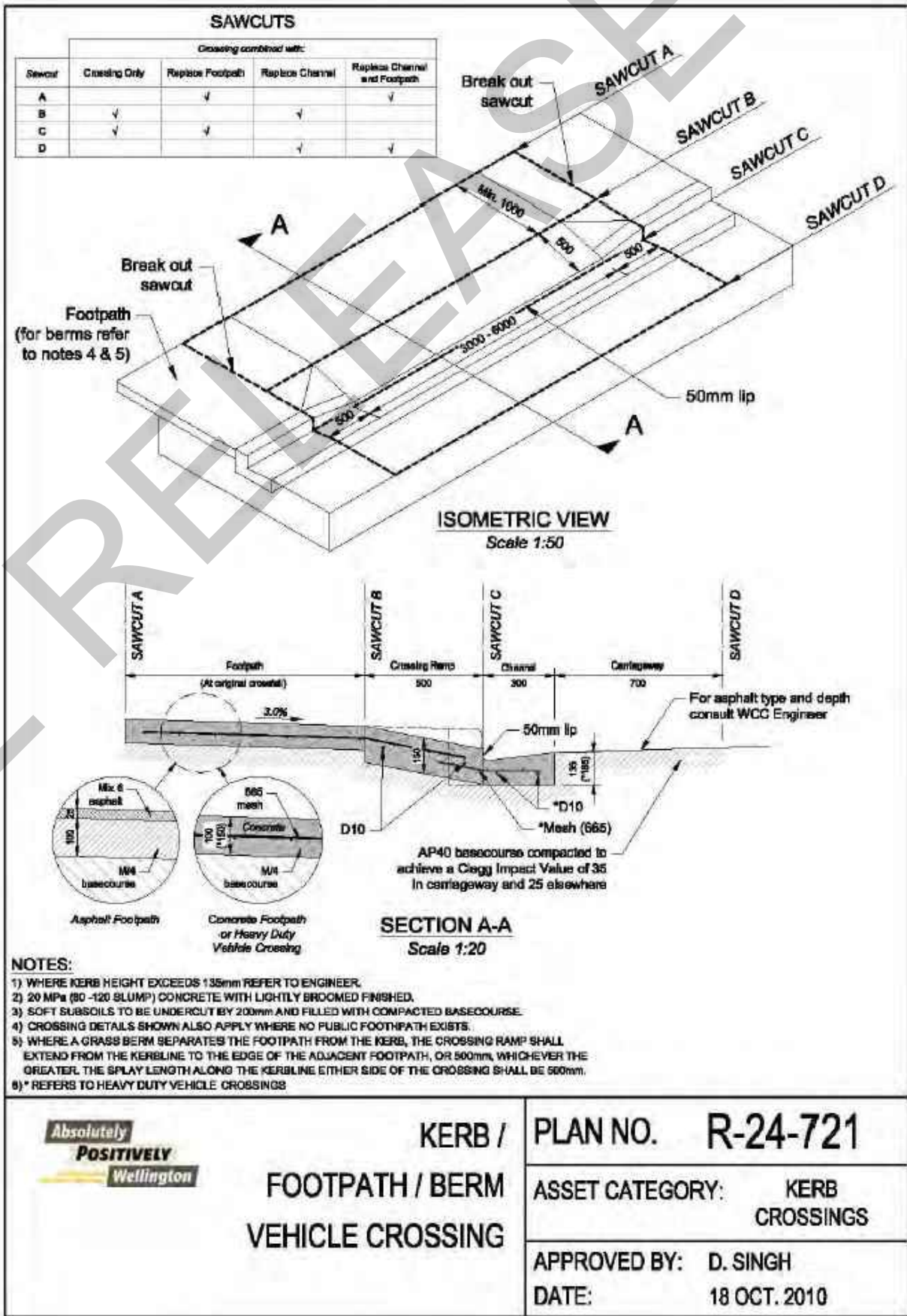


ORIGINAL SIZE A1

**NOTE:**  
SET IN 20MPa CONCRETE BASE.



	<h2>TRAFFIC SIGN SUPPORT SYSTEM</h2>	<b>PLAN NO. R-44-782</b>
		<b>ASSET CATEGORY: TRAFFIC AND PARKING SIGNS</b>
		<b>APPROVED BY: D. SINGH</b> <b>DATE: 14 DEC. 2007</b>



- NOTES:**
- WHERE KERB HEIGHT EXCEEDS 135mm REFER TO ENGINEER.
  - 20 MPa (80-120 SLUMP) CONCRETE WITH LIGHTLY BROOMED FINISH.
  - SOFT SUBSOILS TO BE UNDERCUT BY 200mm AND FILLED WITH COMPACTED BASECOURSE.
  - CROSSING DETAILS SHOWN ALSO APPLY WHERE NO PUBLIC FOOTPATH EXISTS.
  - WHERE A GRASS BERM SEPARATES THE FOOTPATH FROM THE KERB, THE CROSSING RAMP SHALL EXTEND FROM THE KERBLINE TO THE EDGE OF THE ADJACENT FOOTPATH, OR 500mm, WHICHEVER THE GREATER. THE SPREAD LENGTH ALONG THE KERBLINE EITHER SIDE OF THE CROSSING SHALL BE 500mm.
  - \* REFERS TO HEAVY DUTY VEHICLE CROSSINGS

	<b>KERB / FOOTPATH / BERM VEHICLE CROSSING</b>	<b>PLAN NO. R-24-721</b>
		<b>ASSET CATEGORY: KERB CROSSINGS</b>
		<b>APPROVED BY: D. SINGH</b> <b>DATE: 18 OCT. 2010</b>

REV	DESCRIPTION	CD	BH	CHK	CDK	APP	DATE
1	FOR CONSTRUCTION						04/23

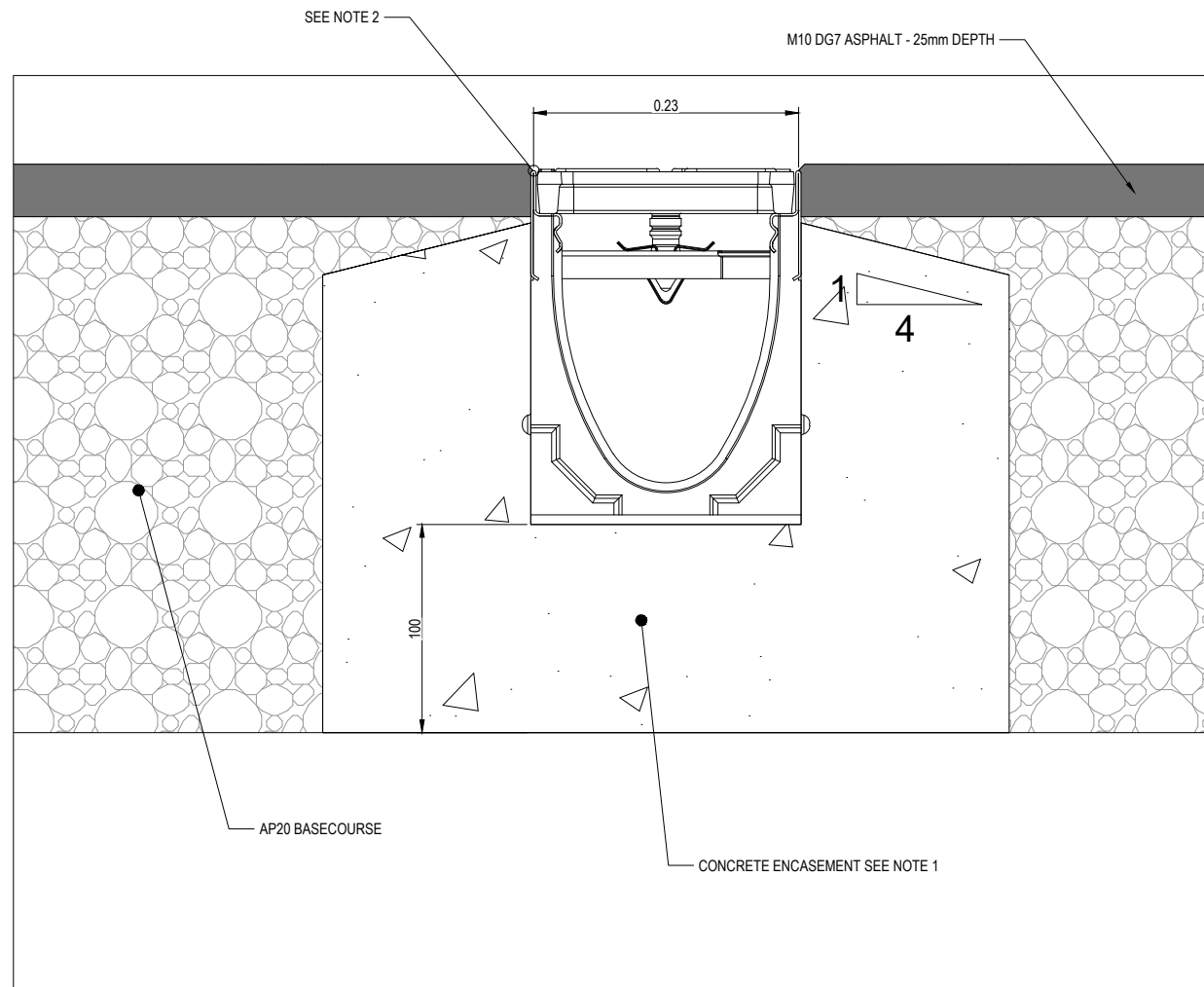
SURVEYED		
DESIGNED	COBUS DE KOCK	01/22
DRAWN	SHAY GRAHAM	01/22
CAD REVIEW	BRODY HALLIDAY	13.04.23
DESIGN CHECK	MATT HOPKINS	13.04.23
DESIGN REVIEW	ANDREW MCLEOD	13.04.23
APPROVED	COBUS DE KOCK	13.04.23

		Client: GWRC - MINOR CIVIL WORKS PARK ROAD BUS STOP	Status Stamp: <b>FOR CONSTRUCTION</b> Date Stamp: <b>13.04.2023</b>
		Drawing No.: 310205101-02-001-C030	Scales: NOT TO SCALE Rev: 1



ORIGINAL SIZE A1

DO NOT SCALE - IF IN DOUBT, ASK



ACODRAIN SPECIFICATION CLAUSE  
KLASSIKDRAIN K100 - LOAD CLASS A-B

GENERAL  
THE SURFACE DRAINAGE SYSTEM SHALL BE ACO'S KLASSIKDRAIN K100 POLYMER CONCRETE V-PROFILE CHANNEL SYSTEM WITH GALVANISED STEEL EDGE RAILS AS MANUFACTURED BY ACO.

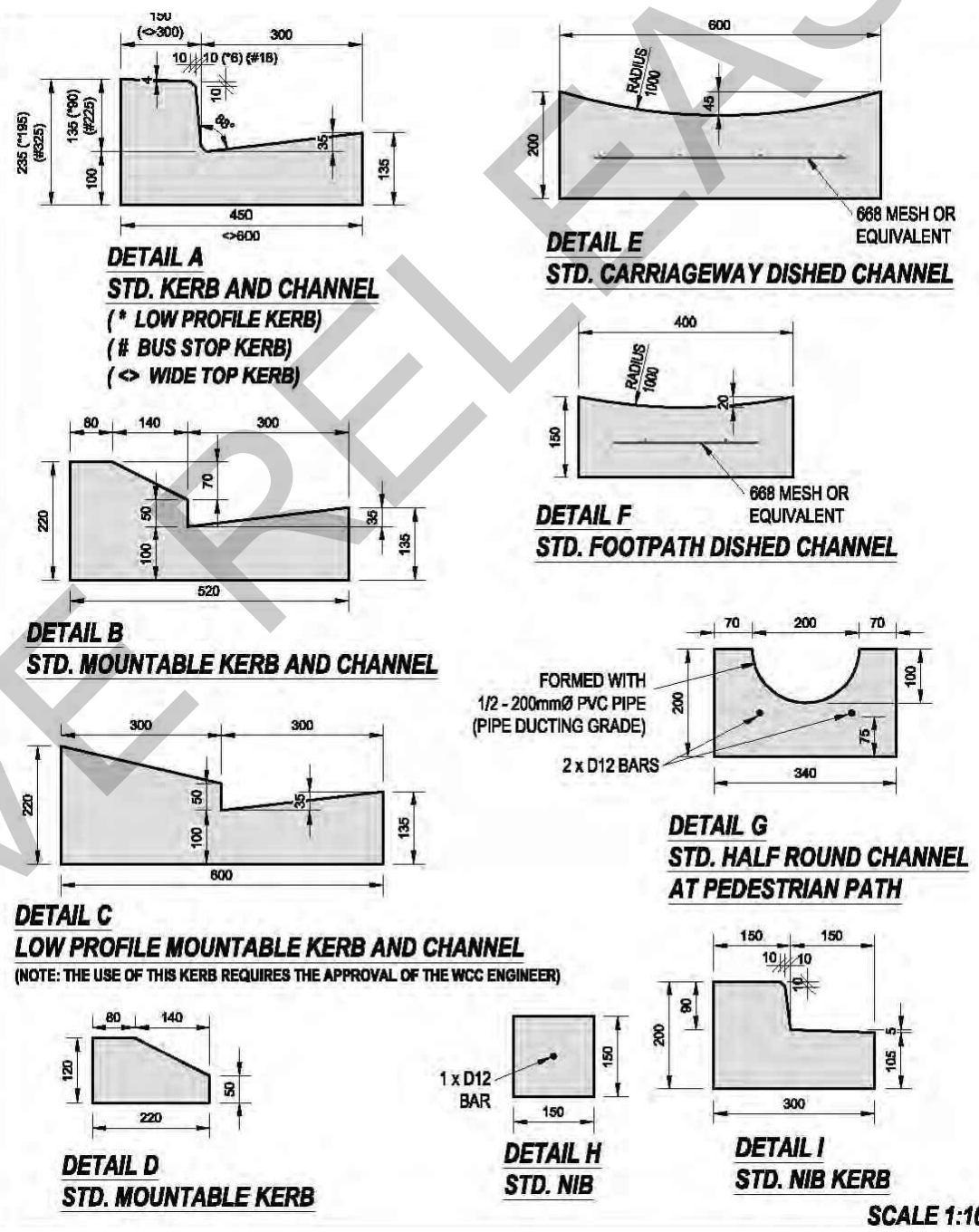
MATERIALS  
K100 CHANNELS SHALL BE MANUFACTURED FROM POLYESTER RESIN POLYMER CONCRETE WITH AN INTEGRALLY CAST-IN GALVANISED STEEL EDGE RAIL. PROPERTIES OF POLYMER CONCRETE WILL BE AS FOLLOWS WITH SUPPORTING DOCUMENTATION:

COMPRESSIVE STRENGTH:	98 MPa
FLEXURAL STRENGTH:	26 MPa
TENSILE STRENGTH:	14 MPa
WATER ABSORPTION:	0.07%
FROST PROOF:	YES
COEFFICIENT OF EXPANSION/CONTRACTION:	2.02x10 <sup>-6</sup> /°c
WATER VAPOUR TRANSMISSION:	0.0364g/m <sup>2</sup>
NON FLAMMABLE:	YES
COEFFICIENT OF ROUGHNESS (MANNINGS):	N=0.011
RESISTANT TO WEATHERING:	YES
DILUTE ACID AND ALKALI RESISTANT:	YES
SF SEALANT GROOVE	YES

CHANNELS  
K100 CHANNEL SHALL BE 100mm NOMINAL INTERNAL WIDTH WITH AN OVERALL WIDTH OF 130mm. CHANNEL INVERT SHALL HAVE A V-PROFILE TO ALLOW EFFICIENT DRAINAGE. K100 SLOPED CHANNELS SHALL HAVE A BUILT-IN SLOPE OF 0.5%. ALL CHANNELS SHALL BE INTERLOCKING WITH A MALE/FEMALE JOINT.

GRATES  
INSERT SPECIFICATION FOR THE SELECTED GRATE. REFER TO THE RELEVANT ACO SPECIFICATION INFORMATION SHEET, CLICK [WWW.ACODRAIN.COM.AU/RESOURCES](http://WWW.ACODRAIN.COM.AU/RESOURCES).

INSTALLATION  
THE COMPLETE DRAINAGE SYSTEM SHALL BE BY ACO AND TO BE INSTALLED FOR ITS INTENDED PURPOSE. ANY DEVIATION OR PARTIAL USE OF THE SPECIFIED SYSTEM AND/OR IMPROPER INSTALLATION WILL VOID ALL WARRANTIES PROVIDED BY ACO.



- ACODRAIN NOTES
- SPECIFIC SITE CONDITIONS MAY REQUIRE AN INCREASE IN CONCRETE ENCASEMENT DIMENSIONS AND/OR REINFORCEMENT. IT IS THE CUSTOMER'S RESPONSIBILITY TO ENSURE THE CONCRETE ENCASEMENT IS DESIGNED FOR THE APPLICATION. A MINIMUM CONCRETE STRENGTH OF 25MPA IS RECOMMENDED. THE CONCRETE SHOULD BE VIBRATED TO ELIMINATE AIR POCKETS. ENGINEERING ADVICE MAY BE REQUIRED.
  - THE FINISHED LEVEL OF THE ASPHALT MUST BE APPROXIMATELY 3MM ABOVE THE TOP OF THE CHANNEL EDGE.
  - HAUNCH SLOPES AWAY AT A RATIO OF 1:4 OR APPROXIMATELY 15°.
  - FOR FURTHER DETAILS, REFER TO ACO'S DESIGN & SITE INSTALLATION FILES AT [WWW.ACODRAIN.COM/RESOURCES](http://WWW.ACODRAIN.COM/RESOURCES).

1	FOR CONSTRUCTION	CD	BH	CDK	04 23
REV		DRN	CHK	APP	DATE
	REVISIONS				

SURVEYED		
DESIGNED	COBUS DE KOCK	01/22
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DESIGN CHECK	MATT HOPKINS	13.04.23
DESIGN REVIEW	ANDREW MCLEOD	13.04.23
APPROVED	COBUS DE KOCK	13.04.23
PROF REGISTRATION:		

Client:

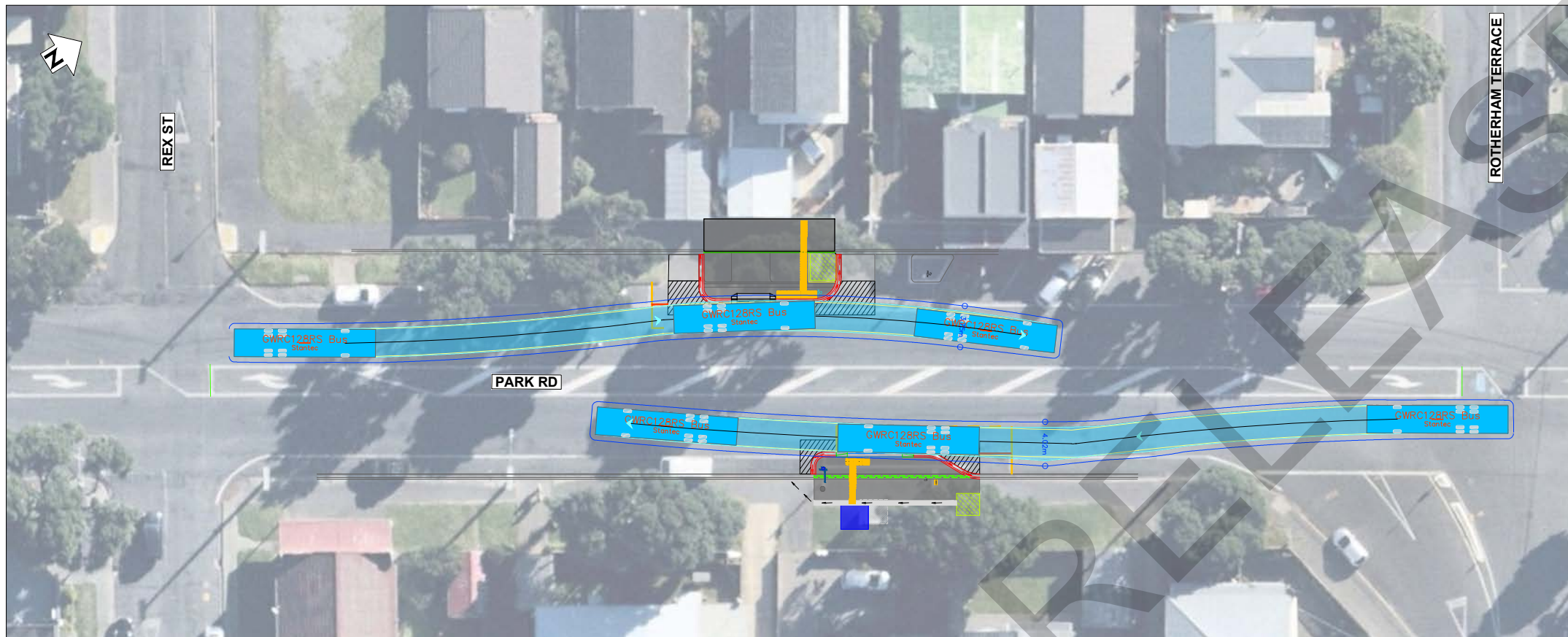
greater WELLINGTON REGIONAL COUNCIL  
Te Pane Mātua Talao

GWRC - MINOR CIVIL WORKS  
PARK ROAD BUS STOP

STANDARD DETAILS  
SHEET 2 OF 2

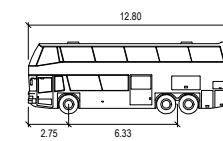
Status Stamp	<b>FOR CONSTRUCTION</b>	
Date Stamp	13.04.2023	
Scales	NOT TO SCALE	
Drawing No.	310205101-02-001-C031	Rev. 1





**LEGEND**

- 0.5m SAFETY ENVELOPE
- VEHICLE ENVELOPE
- FRONT AXLE TRACKING
- VEHICLE CENTRELINE

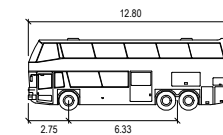
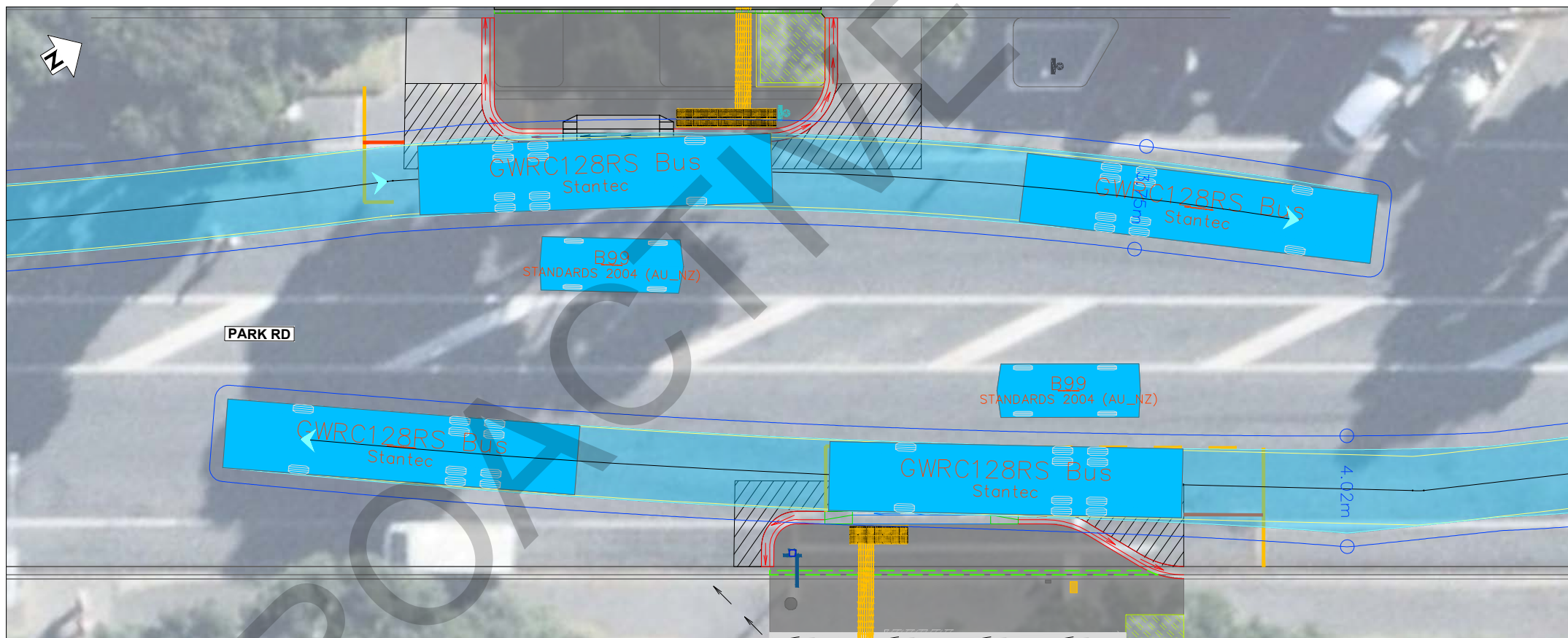


**GWRC128RS Bus**

meters

- Width : 2.50
- Track : 2.50
- Lock to Lock Time : 6.0
- Steering Angle : 35.9

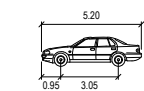
**VEHICLE TRACKING LAYOUT 1**  
SCALE 1 : 250 (A1), 1 : 500 (A3)



**GWRC128RS Bus**

meters

- Width : 2.50
- Track : 2.50
- Lock to Lock Time : 6.0
- Steering Angle : 35.9



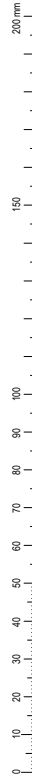
**B99**

meters

- Width : 1.94
- Track : 1.94
- Lock to Lock Time : 6.0
- Steering Angle : 33.9

**VEHICLE TRACKING LAYOUT 2**  
SCALE 1 : 100 (A1), 1 : 200 (A3)

DO NOT SCALE - IF IN DOUBT, ASK



ORIGINAL SIZE A1

REV	DESCRIPTION	CD	BH	CDK	DATE
1	FOR CONSTRUCTION	DRN	CHK	APP	04 23

SURVEYED	DESIGNED	DRAWN	CAD REVIEW	DESIGN CHECK	DESIGN REVIEW	APPROVED
	COBUS DE KOCK	SHAY GRAHAM	BRODY HALLIDAY	MATT HOPKINS	ANDREW MCLEOD	COBUS DE KOCK
	01/22	01/22	13.04.23	13.04.23	13.04.23	13.04.23



Client: GWRC - MINOR CIVIL WORKS  
PARK ROAD BUS STOP

Vehicle TRACKING

Status Stamp	<b>FOR CONSTRUCTION</b>
Date Stamp	<b>13.04.2023</b>
Scales	AS SHOWN
Drawing No.	<b>310205101-02-001-C040</b>
Rev.	<b>1</b>